

PH0666939

UNITED STATES DEPARTMENT OF THE INTERIOR
NATIONAL PARK SERVICE

FOR NPS USE ONLY
RECEIVED APR 12 1978
DATE ENTERED SEP 20 1978

NATIONAL REGISTER OF HISTORIC PLACES
INVENTORY -- NOMINATION FORM

SEE INSTRUCTIONS IN HOW TO COMPLETE NATIONAL REGISTER FORMS
TYPE ALL ENTRIES -- COMPLETE APPLICABLE SECTIONS

1 NAME

HISTORIC

John Bright #01 Iron Bridge

AND/OR COMMON

2 LOCATION

STREET & NUMBER

Havensport Road

CITY, TOWN

2 miles/northeast of Carroll

VICINITY OF

Lancaster

NOT FOR PUBLICATION

CONGRESSIONAL DISTRICT

#10

STATE

Ohio

CODE

039

COUNTY

Fairfield

CODE

045

3 CLASSIFICATION

CATEGORY	OWNERSHIP	STATUS	PRESENT USE
<input type="checkbox"/> DISTRICT	<input checked="" type="checkbox"/> PUBLIC	<input checked="" type="checkbox"/> OCCUPIED	<input type="checkbox"/> AGRICULTURE <input type="checkbox"/> MUSEUM
<input type="checkbox"/> BUILDING(S)	<input type="checkbox"/> PRIVATE	<input type="checkbox"/> UNOCCUPIED	<input type="checkbox"/> COMMERCIAL <input type="checkbox"/> PARK
<input checked="" type="checkbox"/> STRUCTURE	<input type="checkbox"/> BOTH	<input type="checkbox"/> WORK IN PROGRESS	<input type="checkbox"/> EDUCATIONAL <input type="checkbox"/> PRIVATE RESIDENCE
<input type="checkbox"/> SITE	PUBLIC ACQUISITION	ACCESSIBLE	<input type="checkbox"/> ENTERTAINMENT <input type="checkbox"/> RELIGIOUS
<input type="checkbox"/> OBJECT	<input type="checkbox"/> IN PROCESS	<input type="checkbox"/> YES: RESTRICTED	<input type="checkbox"/> GOVERNMENT <input type="checkbox"/> SCIENTIFIC
	<input type="checkbox"/> BEING CONSIDERED	<input checked="" type="checkbox"/> YES: UNRESTRICTED	<input type="checkbox"/> INDUSTRIAL <input checked="" type="checkbox"/> TRANSPORTATION
		<input type="checkbox"/> NO	<input type="checkbox"/> MILITARY <input type="checkbox"/> OTHER:

4 OWNER OF PROPERTY

NAME

Fairfield County

STREET & NUMBER

CITY, TOWN

Lancaster

VICINITY OF

STATE

Ohio

5 LOCATION OF LEGAL DESCRIPTION

COURTHOUSE,
REGISTRY OF DEEDS, ETC.

Court House

STREET & NUMBER

CITY, TOWN

Lancaster

STATE

Ohio

6 REPRESENTATION IN EXISTING SURVEYS

TITLE

Ohio Historic Inventory

DATE

Dec. 1976

FEDERAL STATE COUNTY LOCAL

DEPOSITORY FOR
SURVEY RECORDS

Ohio Historical Society

CITY, TOWN

Columbus

STATE

Ohio

7 DESCRIPTION

CONDITION		CHECK ONE	CHECK ONE
<input type="checkbox"/> EXCELLENT	<input type="checkbox"/> DETERIORATED	<input checked="" type="checkbox"/> UNALTERED	<input checked="" type="checkbox"/> ORIGINAL SITE
<input type="checkbox"/> GOOD	<input type="checkbox"/> RUINS	<input type="checkbox"/> ALTERED	<input type="checkbox"/> MOVED DATE _____
<input checked="" type="checkbox"/> FAIR	<input type="checkbox"/> UNEXPOSED		

DESCRIBE THE PRESENT AND ORIGINAL (IF KNOWN) PHYSICAL APPEARANCE

The structure under consideration is an unusual one-span steel eye-bar suspension bridge spanning Poplar Creek on the Havensport Road (township road 263) 2 miles northeast of Carroll in Liberty Township, Fairfield County, Ohio.

This old structure is known as the John Bright #01 Bridge and was built by the Hocking Valley Bridge Works, probably in the 1880's. Proprietor of the Hocking Valley Bridge Works at that time was August Borneman, well-known builder of both wooden and steel bridges or a combination of both. An excellent example of such a combination is the nearby John Bright #02 Covered Bridge, which is now listed on the National Register. John Bright #01 Bridge is an all steel span except for the floor. The bridge measures 90' overall with a clear span of 88'. The roadway width is 14' and the overall width is 16'. The bridge end posts are 2-6" steel channels riveted together to form box beams that are fastened to 2' square reinforced steel plates on top of the abutments. There are 8 truss verticals (or intermediate posts) which are fabricated box beams consisting of 2 steel channels 6" wide with 1 3/4" ribs and with steel bar lattices riveted to the sides. These lattice sides measure 9 1/4". These truss verticals are of varying lengths from 4' 5" to 12' 7" in the middle. These truss members are fastened to 2 steel eye-bar suspension rods, one on each side of the trusses. At the same joint on each truss vertical where the truss is fastened to the suspension rod, there is a suspension hanger which is fastened to the steel floor joist directly below each truss vertical. These suspension hangers measure from 9' 5" on the ends to 1' 3" in the middle. Between the truss verticals are crossed 1" steel tension rods with turnbuckles. This same arrangement of tension rods is used between the 8 joists under the bridge and between the lateral struts which are between the top chords of the bridge. These upper chords are fabricated steel channels. There are now lower chords, but there are 8 7" steel I beams with 3" plates running longitudinally from abutment to abutment. The 8 steel joists are each 9" with 3" plates. Total height of the bridge truss is 14'. The abutments are cut-stone and the south abutment shows remnants of a concrete reinforcement at the base. The flow of the stream is against this south abutment. The bridge floor consists of 2" x 4" timbers laid on edge.

The John Bright #01 Bridge has changed little in appearance over the years. It was recently painted with shiny aluminum paint. The surface of the floor is now covered with a layer of asphalt, applied no doubt when road resurfacing was done in the area. Each end post is topped with an ornamental steel urn and there is ornamental lacy ironwork trim in each portal corner. A sign over the north portal says "Hocking Valley Bridge Works, Lancaster, Ohio". A small plate near the top of each steel end post gives this same information. The steel used in the bridge came from Carnegie Steel. Under the center of the bridge is a wooden support consisting of 2-2" x 8" with 2-2" x 4" crossed between them. This is undoubtedly a later addition.

8 SIGNIFICANCE

PERIOD	AREAS OF SIGNIFICANCE -- CHECK AND JUSTIFY BELOW			
<input type="checkbox"/> PREHISTORIC	<input type="checkbox"/> ARCHEOLOGY-PREHISTORIC	<input type="checkbox"/> COMMUNITY PLANNING	<input type="checkbox"/> LANDSCAPE ARCHITECTURE	<input type="checkbox"/> RELIGION
<input type="checkbox"/> 1400-1499	<input type="checkbox"/> ARCHEOLOGY-HISTORIC	<input type="checkbox"/> CONSERVATION	<input type="checkbox"/> LAW	<input type="checkbox"/> SCIENCE
<input type="checkbox"/> 1500-1599	<input type="checkbox"/> AGRICULTURE	<input type="checkbox"/> ECONOMICS	<input type="checkbox"/> LITERATURE	<input type="checkbox"/> SCULPTURE
<input type="checkbox"/> 1600-1699	<input type="checkbox"/> ARCHITECTURE	<input type="checkbox"/> EDUCATION	<input type="checkbox"/> MILITARY	<input type="checkbox"/> SOCIAL/HUMANITARIAN
<input type="checkbox"/> 1700-1799	<input type="checkbox"/> ART	<input checked="" type="checkbox"/> ENGINEERING	<input type="checkbox"/> MUSIC	<input type="checkbox"/> THEATER
<input checked="" type="checkbox"/> 1800-1899	<input type="checkbox"/> COMMERCE	<input type="checkbox"/> EXPLORATION/SETTLEMENT	<input type="checkbox"/> PHILOSOPHY	<input checked="" type="checkbox"/> TRANSPORTATION
<input type="checkbox"/> 1900-	<input type="checkbox"/> COMMUNICATIONS	<input type="checkbox"/> INDUSTRY	<input type="checkbox"/> POLITICS/GOVERNMENT	<input type="checkbox"/> OTHER (SPECIFY)
		<input type="checkbox"/> INVENTION		

SPECIFIC DATES early 1800's

BUILDER/ARCHITECT Hocking Valley Bridge Works

STATEMENT OF SIGNIFICANCE

The John Bright #01 Bridge is named for the pioneer Bright Family who were among the earliest settlers in this area, coming here in 1832. The entire area was once known as the Bright Settlement. According to Fairfield County historian, Charles Goslin, the Havensport Road was opened in 1831 to provide a route to the Ohio and Erie Canal at Havensport which is about 1½ miles south of this area. Just around the corner from this unique old steel bridge is the unusual old John Bright #2 Bridge, an inverted arch suspension truss covered bridge, also built by August Borneman, proprietor of the Hocking Valley Bridge Works. The covered bridge was built in 1881 and the similarity of construction between the two bridges leads us to believe that Mr. Borneman was still with the Hocking Valley Bridge Works when the steel bridge was built. Mr. Borneman was a well-known bridge-builder in Fairfield County and would build any type of bridge the customer wanted. One of his bridges, now submitted to the National Register and under consideration, is a Howe patent truss. A search of patent records shows us that August Borneman was quite an inventor. He held patents on farm machinery, a composite pier for bridges, and one truss bridge. This truss bridge was a truss-leg bedstead type of bridge, mostly used for smaller streams. The truss drawing was used on the Hocking Valley Bridge Works stationary as part of the letterhead. Our patent search has failed to turn up a patent for this type of bridge under consideration here. We feel that it is entirely possible that this was an experimental model tried by Mr. Borneman and perhaps never used again. So far, no other bridge like it has been found here in Ohio, but it does bear a striking similarity to the nearby John Bright #02 Covered Bridge and the famous covered bridge at Germantown, Ohio.

We feel that the old John Bright #01 Bridge is an outstanding example of early steel bridge design here in Ohio and well-worthy of preservation as an outstanding example of the creativity of August Borneman, bridge-builder and inventor.

9 MAJOR BIBLIOGRAPHICAL REFERENCES

- Allen, Richard S., Covered Bridges of the Middle West, Stephan Greene Press, 1970, p.49.
 Deibler, Dan Grove, Metal Truss Bridges in Virginia, Part 1, 1975, p. 17.
 Record Books of the Southern Ohio Covered Bridge Association, Fairfield County, Vol.2.
 Ketcham, Bryan E., Covered Bridges on the Byways of Ohio, Oxford Printing Co., p.55, 1969.

10 GEOGRAPHICAL DATA

ACREAGE OF NOMINATED PROPERTY less than one

UTM REFERENCES

A	1 7	3 5 6 7 2 0	4 4 0 9 9 4 0	B			
	ZONE	EASTING	NORTHING		ZONE	EASTING	NORTHING
C				D			

VERBAL BOUNDARY DESCRIPTION Beginning at the western most point of the northwestern wingwall, then east to the eastern most point of the northeastern wingwall, then approximately 100 feet to the eastern most point of the southeastern wingwall, then west to the western most point of the southwestern wingwall, then north approximately 100 feet to the place of beginning.

LIST ALL STATES AND COUNTIES FOR PROPERTIES OVERLAPPING STATE OR COUNTY BOUNDARIES

STATE	CODE	COUNTY	CODE

11 FORM PREPARED BY

NAME / TITLE

Miriam F. Wood

May 30, 1976

DATE

ORGANIZATION

STREET & NUMBER

3155 Whitehead Road

TELEPHONE

614-279-9576

CITY OR TOWN

Columbus

STATE

Ohio

12 STATE HISTORIC PRESERVATION OFFICER CERTIFICATION

THE EVALUATED SIGNIFICANCE OF THIS PROPERTY WITHIN THE STATE IS:

NATIONAL

STATE

LOCAL

As the designated State Historic Preservation Officer for the National Historic Preservation Act of 1966 (Public Law 89-665), I hereby nominate this property for inclusion in the National Register and certify that it has been evaluated according to the criteria and procedures set forth by the National Park Service.

STATE HISTORIC PRESERVATION OFFICER SIGNATURE



TITLE State Historic Preservation Officer

DATE 7/3/78

FOR NPS USE ONLY

I HEREBY CERTIFY THAT THIS PROPERTY IS INCLUDED IN THE NATIONAL REGISTER

R. B. Retting

DATE

9/20/78

DIRECTOR, OFFICE OF ARCHEOLOGY AND HISTORIC PRESERVATION

KEEPER OF THE NATIONAL REGISTER

ATTEST:

William Lebowich

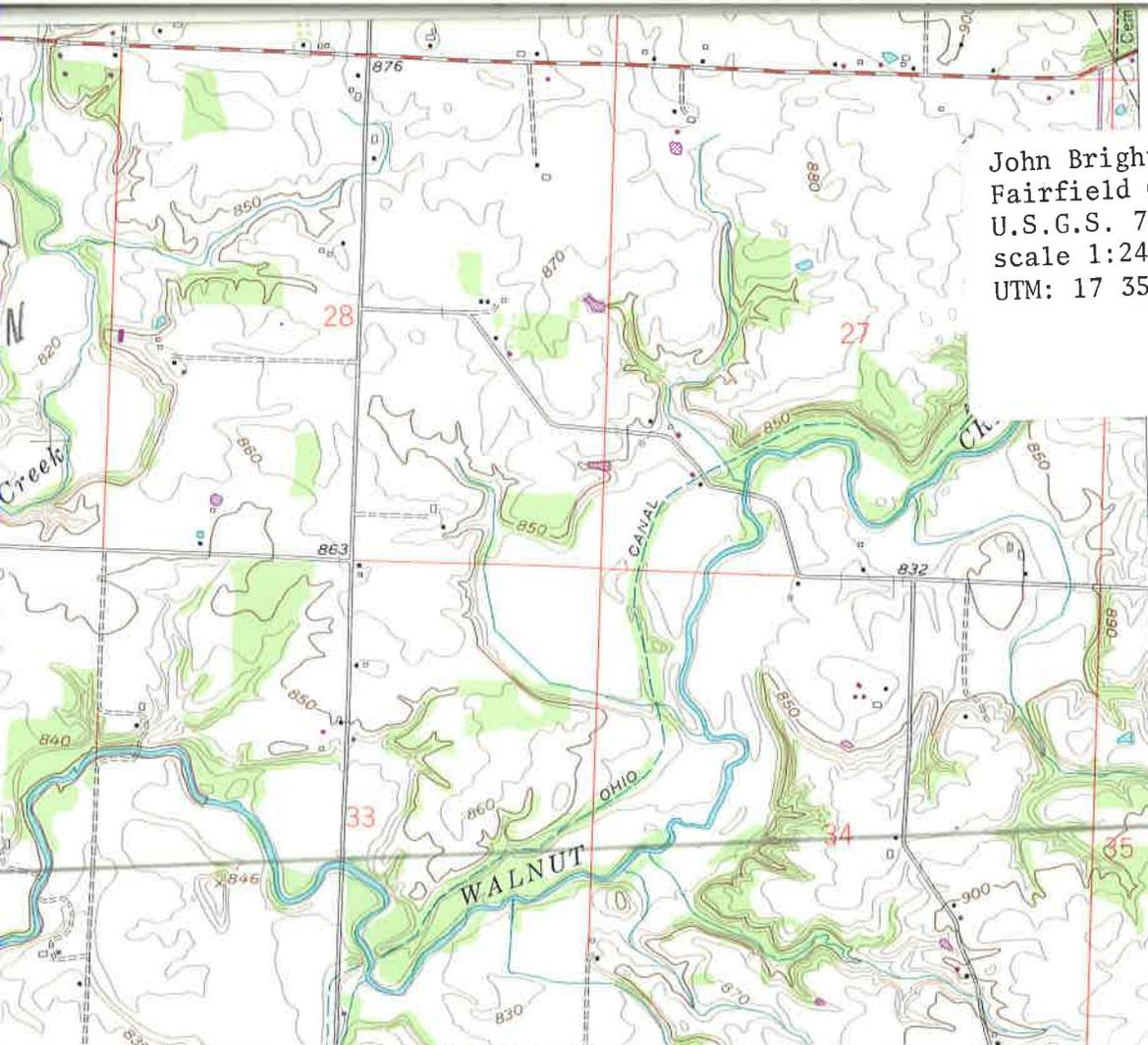
DATE

Sept 18, 1978

KEEPER OF THE NATIONAL REGISTER







John Bright #01 Iron Bridge
Fairfield Co., Ohio
U.S.G.S. 7.5' Carroll Quad.
scale 1:24000 1958, 1970
UTM: 17 356/720 4409/940

John Bright #01 Iron Bridge
2 northeast of Carroll
Fairfield County, Ohio

UTM 17
356/720
4409/940

T. 16 N.