



**WALDO-HANCOCK BRIDGE ADDITIONAL  
DOCUMENTATION (REMOVAL)**

Name of Property

**WALDO AND HANCOCK  
COUNTIES, MAINE**

County and State

**4. National Park Service Certification**

I hereby certify that this property is:

- entered in the National Register
- determined eligible for the National Register
- determined not eligible for the National Register
- removed from the National Register
- other (explain:)

*Mr. Edison H. Beall*  
\_\_\_\_\_  
Signature of the Keeper

\_\_\_\_\_  
Date of Action

**5. Classification**

**Ownership of Property**

(Check as many boxes as apply.)

- Private
- Public – Local
- Public – State
- Public – Federal

**Category of Property**

(Check only **one** box.)

- Building(s)
- District
- Site
- Structure
- Object

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## 8. Statement of Significance

### Statement of Significance Summary Paragraph

(Provide a summary paragraph that includes level of significance, applicable criteria, justification for the period of significance, and any applicable criteria considerations.) (Refer to photographs)

On June 20<sup>th</sup>, 1985 the Waldo-Hancock Bridge (NR: 85001267) was listed in the National Register of Historic Places. Spanning the Penobscot River between Prospect, in Waldo County and Verona in Hancock County, this high suspension bridge was both an engineering and visual landmark. The 2,040 foot steel bridge was constructed in 1931 by the nationally recognized engineering firm of Robinson & Steinman of New York. Shortly after it was completed the bridge received the Annual Award of Merit "Most Beautiful Steel Bridge" *Class B* from the American Institute of Steel Construction. Due to advanced deterioration in the cables, the Maine Department of Transportation was compelled to erect a new bridge in 2006 and close the Waldo Hancock Bridge that same year. The bridge was dismantled beginning in November 2012, with the final removal of the steel towers completed in June 2013. This additional documentation is submitted in order to remove the Waldo-Hancock Bridge from the National Register as per 36 CFR 60.15 (a)(1) because "the property has ceased to meet the criteria for listing in the National Register because the qualities which caused it to be originally listed have been lost or destroyed..." For contemporary accounts of the demolition please see the news articles referenced in the bibliography.

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**Narrative Statement of Significance** (Provide at least **one** paragraph for each area of significance.)

N/A

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**Developmental history/additional historic context information** (If appropriate.)

N/A

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**9. Major Bibliographical References**

**Bibliography** (Cite the books, articles, and other sources used in preparing this form.)

- Bennett, Kevin. "Remembering Waldo-Hancock Bridge." *Bangor Daily news*, April 17, 2013.  
Miller, Kevin. "Waldo-Hancock bridge salvage set this fall." *Bangor Daily News*, February 15, 2012: B1, B4.  
Moretto, Mario. "81-year-old span being taken apart section by section." *Bangor Daily news*, January 7, 2013: 1, A2.  
Moretto, Mrio. "Cutting the last cables: Lines drop from old Waldo-Hancock Bridge." *Bangor Daily News*, May 17, 2013: B1, B6.  
Osborn, Jennifer. "Waldo-Hancock Bridge Removal to Begin in 2012." *Ellsworth American*, December 22, 2011.

**Previous documentation on file (NPS):**

- preliminary determination of individual listing (36 CFR 67) has been requested
- previously listed in the National Register
- previously determined eligible by the National Register
- designated a National Historic Landmark
- recorded by Historic American Buildings Survey # \_\_\_\_\_
- recorded by Historic American Engineering Record # HAER ME,5-BUCK.V,1
- recorded by Historic American Landscape Survey # \_\_\_\_\_

**Primary location of additional data:**

- State Historic Preservation Office
- Other State agency
- Federal agency
- Local government
- University
- Other

Name of repository: \_\_\_\_\_

**Historic Resources Survey Number (if assigned):** \_\_\_\_\_

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## 10. Geographical Data

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## 11. Form Prepared By

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date: 3 September 2013

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## Additional Documentation

Submit the following items with the completed form:

- **Maps:** A **USGS map** or equivalent (7.5 or 15 minute series) indicating the property's location.
- **Sketch map** for historic districts and properties having large acreage or numerous resources. Key all photographs to this map.
- **Additional items:** (Check with the SHPO, TPO, or FPO for any additional items.)

## Photographs

Submit clear and descriptive photographs. The size of each image must be 1600x1200 pixels (minimum), 3000x2000 preferred, at 300 ppi (pixels per inch) or larger. Key all photographs to the sketch map. Each photograph must be numbered and that number must correspond to the photograph number on the photo log. For simplicity, the name of the photographer, photo date, etc. may be listed once on the photograph log and doesn't need to be labeled on every photograph.

**Paperwork Reduction Act Statement:** This information is being collected for applications to the National Register of Historic Places to nominate properties for listing or determine eligibility for listing, to list properties, and to amend existing listings. Response to this request is required to obtain a benefit in accordance with the National Historic Preservation Act, as amended (16 U.S.C.460 et seq.).

**Estimated Burden Statement:** Public reporting burden for this form is estimated to average 100 hours per response including time for reviewing instructions, gathering and maintaining data, and completing and reviewing the form. Direct comments regarding this burden estimate or any aspect of this form to the Office of Planning and Performance Management, U.S. Dept. of the Interior, 1849 C. Street, NW, Washington, DC.

EX

# Waldo-Hancock Bridge Removal to Begin in 2012

BY JENNIFER OSBORN  
[josborn@ellsworthamerican.com](mailto:josborn@ellsworthamerican.com)  
VERONA ISLAND — What goes up must come down and so it is with the 1931 Waldo-Hancock Bridge, which is scheduled for removal in 2012.

The Maine Department of Transportation is in the final planning stage for removal of the bridge, which was replaced in 2006 by the Penobscot Narrows Bridge.

Doug Coombs, assistant project manager in the DOT's bridge program, said the project will be put out to bid in June.

"We're working on final plans and specifications for the removal," Coombs said.

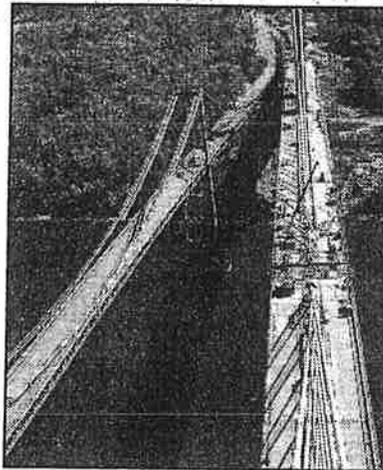
The project is estimated to cost \$7,598,000 with the federal government providing 80 percent of the funds and the state 20 percent, Coombs said.

Coombs anticipates the project starting in August 2012 and finishing in June 2013.

But, the actual removing of the bridge won't happen until next fall.

First, leaf-peepers must be able to peep from the Penobscot Narrows observatory. DOT spokesman Ted Talbot said the DOT doesn't want construction vehicles interfering with visitors.

Also, a peregrine falcon must finish nesting atop the Waldo-Han-



FILE PHOTO

**The Waldo-Hancock Bridge, built in 1931 and shown here with the Penobscot Narrows Bridge under construction, is scheduled for removal in 2012.**

cock Bridge before it is moved.

Leaf-peeping and falcon nesting are October activities, so the demolition won't start until November 2012.

"Our plan is to keep the observatory open until the end of October, which is very, very important in our opinion," Coombs said. "That's leaf-peeping season. It's a big deal."

No word yet on where the falcon nest will be moved.

"They're continuing to work on a solution," Talbot said. "There's several options."

Among those are several ledges in the area, he said.

Once the removal work begins, workers will take the bridge apart using barges equipped with cranes.

The deck will be removed first starting from the middle of the bridge, Coombs said. Towers will be stabilized then the cables removed then towers taken out.

The components will become property of the contractor who gets the contract to remove it.

At least two pieces of the Waldo-Hancock will stay in Bucksport.

Coombs said a portion of the bridge will be saved to put on a walking trail in Bucksport.

Also, one of two flagpoles, which are on top of the bridge, will go to Bucksport. The other flagpole will go to Waldo County.

The bridge's two piers will be lit and will stay in the water.

"We're working with the U.S. Coast Guard right now on those," Coombs said. "We have to have a lighting plan for the piers."

The piers are spaced 700 feet apart and are out of the navigation channel, so Coombs doesn't foresee a problem for watercraft avoiding the piers.

[news.fenceviewer.com](http://news.fenceviewer.com)

# Waldo-Hancock bridge salvage set this fall

2/13/12 BDN

BY KEVIN MILLER  
BDN STAFF

VERONA ISLAND — The Waldo-Hancock Bridge that carried generations of motorists across the Penobscot River but now rusts in the shadow of its modern replacement finally has a date with the wrecking crew.

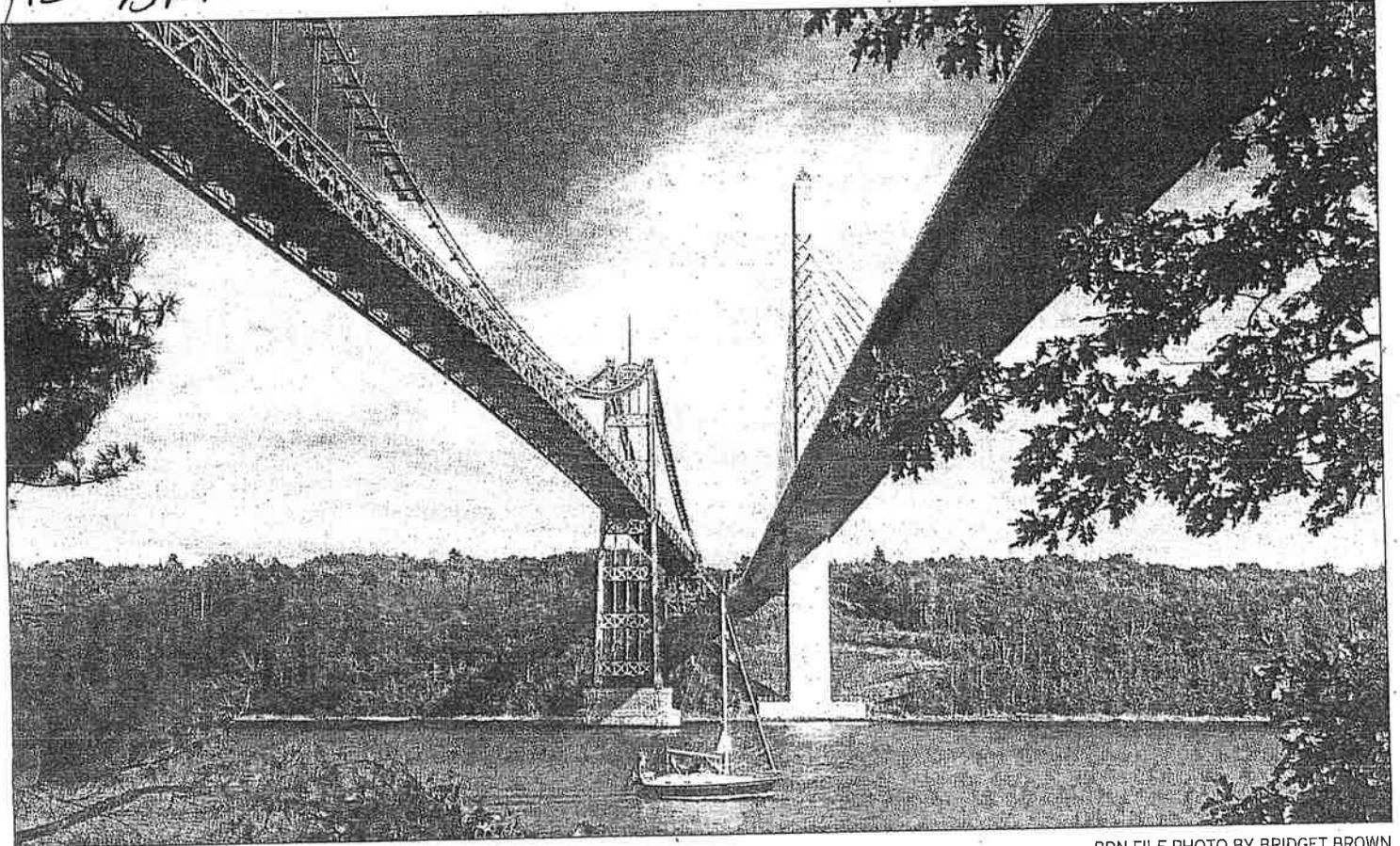
Initial work to remove the 86-year-old suspension bridge linking Verona Island and Prospect is expected to begin this summer, more than five years after the Penobscot Narrows Bridge opened to traffic, according to the Maine Department of Transportation.

But don't expect the old span to come down with a big boom and a dramatic splash.

The \$7.6 million removal project is expected to take nearly a year, with the bulk of the heavy-duty deconstruction occurring next fall and winter in order to minimize effects on endangered fish and birds that live and breed on or below the bridge.

"The plan we have is a salvage plan," said Douglas Coombs, assistant project manager with the Maine DOT. "There will be cranes on barges in the water that will remove the superstructure of the bridge."

State and local officials had hoped to remove the deteriorating bridge not long after the Penobscot Narrows Bridge and Observatory opened in late 2006. But securing the necessary authorizations and funding for the massive project took longer



BDN FILE PHOTO BY BRIDGET BROWN

A sailboat passes between the Waldo-Hancock Bridge (left) and the Penobscot Narrows Bridge and Observatory. The Waldo-Hancock Bridge was built in 1931 and no longer is used.

than anticipated.

When it opened in 1931, the Waldo-Hancock Bridge was the first permanent bridge across the Penobscot south of Bangor and replaced a ferry system for cars. But the 2,040-foot-long span was deteriorating to the

point that, in 2003, state officials announced that the problems were beyond repair and a new bridge was needed.

Nearly a decade later, the old bridge is a rusting skeleton beside the iconic, \$85 million Penobscot Narrows Bridge with

its two, obelisk-like towers and angular system of cables. And from a tourism standpoint, the Waldo-Hancock bridge is in the line of sight of three popular stops for visitors: Fort Knox State Historic Site, the observatory 420 feet above the river in

one of the new bridge's towers and Bucksport's downtown waterfront.

"Taking that old bridge down is going to make a big difference," said Roger Raymond, town manager of Bucksport, *See Bridge, Page B4*

# Bridge

*Continued from Page B1*

which has developed a waterfront walking path overlooking the river and bridges that is popular with both local residents and tourists.

Coombs said the department plans to solicit bids for the project in mid-June with prep work beginning by mid-August or early September.

Work on the bridge itself cannot begin until Oct. 1, however, due to the presence of pairs of peregrine falcons and osprey that have chosen to nest on the old bridge. Peregrine falcons are an endangered species in Maine, so Oct. 1 was set as the start date in order to ensure that any peregrine chicks in the nest had time to develop and leave the nest.

Brad Allen, a bird biologist with the Maine Department of Inland Fisheries and Wildlife, said biologists are still discussing options, including attempting to relocate the nest nearby. Osprey readily accept man-made platform nests, so one will likely be placed along the river in the area, Allen said.

"That gives us a whole nesting season to figure this out," Allen said of the Oct. 1 start date.

Demolition crews also will have to wait until after Nov. 1 to bring in the barges to work on the bridge's superstructure in order to avoid harming endangered shortnose sturgeon that move through that stretch of the river during the summer and fall.

Work on the superstructure is expected to continue through April 2013 with a goal of completing landscaping around the site of the former bridge by June 2013.

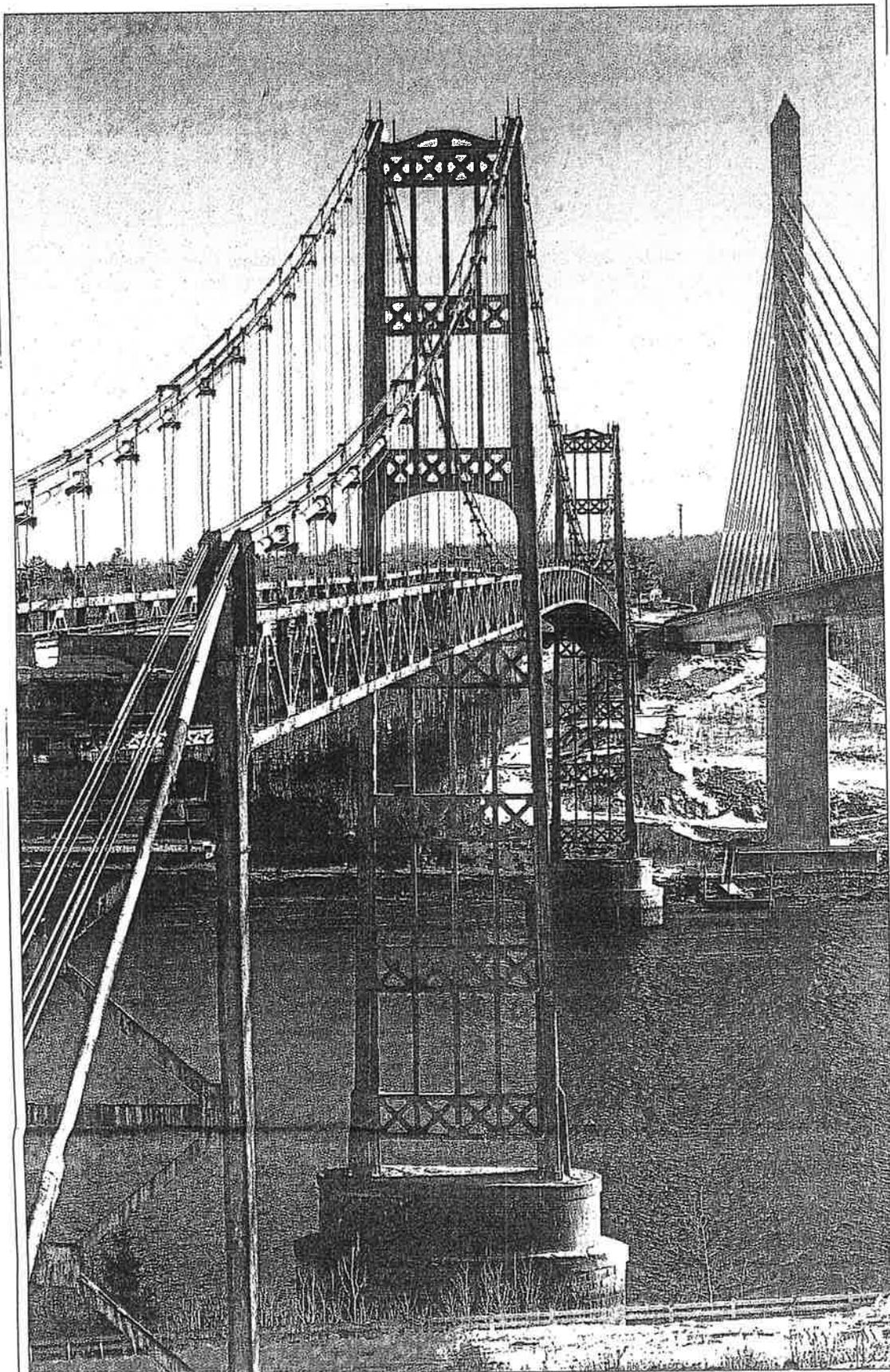
The old bridge footings, or piers, will not be removed, however, so the state is working with the U.S. Coast Guard to design navigational lights to alert boaters to the piers, which are located 700 feet apart.

The Maine Bureau of Parks and Lands also is developing plans to use the riverfront area now occupied by the old bridge.

Mike Leighton, regional manager for the bureau, said staff members hope to build new trails to and from Fort Knox, picnic areas and potentially a group shelter located on top of one of the large, concrete blocks that is part of the suspension bridge's counterbalance system.

The federal government is paying 80 percent of the estimated \$7.6 million bridge removal costs with the state chipping in the remaining 20 percent, Coombs said.

## Waldo-Hancock Bridge demolition gets under way



BDN PHOTO BY KEVIN BENNETT

The Waldo-Hancock Bridge on Saturday, from the Prospect side. The Maine Department of Transportation is in the process of removing sections of the bridge over the next seven months.

# 81-year-old span being taken apart section by section

BY MARIO MORETTO  
BDN STAFF

Watch the video  
[bangordailynews.com](http://bangordailynews.com)

VERONA ISLAND — Two months after demolition prep began, pieces of the historic Waldo-Hancock Bridge are beginning to disappear.

A barge from Staten Island, N.Y., accompanied by two tugboats, made its way through Bucksport on Saturday afternoon en route to Brewer, where it was to pick up a huge crane integral to the removal of the 81-year-old suspension bridge.

Observant motorists will

have noticed a large section of the bridge's deck, at the Prospect side in Waldo County, has been gone since early December. The span of steel and concrete was the first piece of the bridge removed by a demolition crew from S&R Corp. of Lowell, Mass.

The bridge, which spans the Penobscot River between the Hancock County town of Verona Island and the Waldo County town of Prospect, was built in

1931. Because of its deteriorating condition, the state decided to replace the bridge in 2003. The towering new Penobscot Narrows Bridge was completed in 2006, ending the elder structure's 75-year tenure.

Since that first span was removed in December, crews have been cutting the roadway into smaller pieces to facilitate their removal in the coming months. Taking down the driving surface is the first in a three-phase demolition plan expected to be finished by the end of July.

See Bridge, Page A2

# Bridge

Continued from Page A1

The bridge's demolition comes more than five years after its replacement, the Penobscot Narrows Bridge, was opened to motorists in 2006. S&R submitted the winning bid to Maine Department of Transportation in August; demolition and debris removal for \$5.35 million.

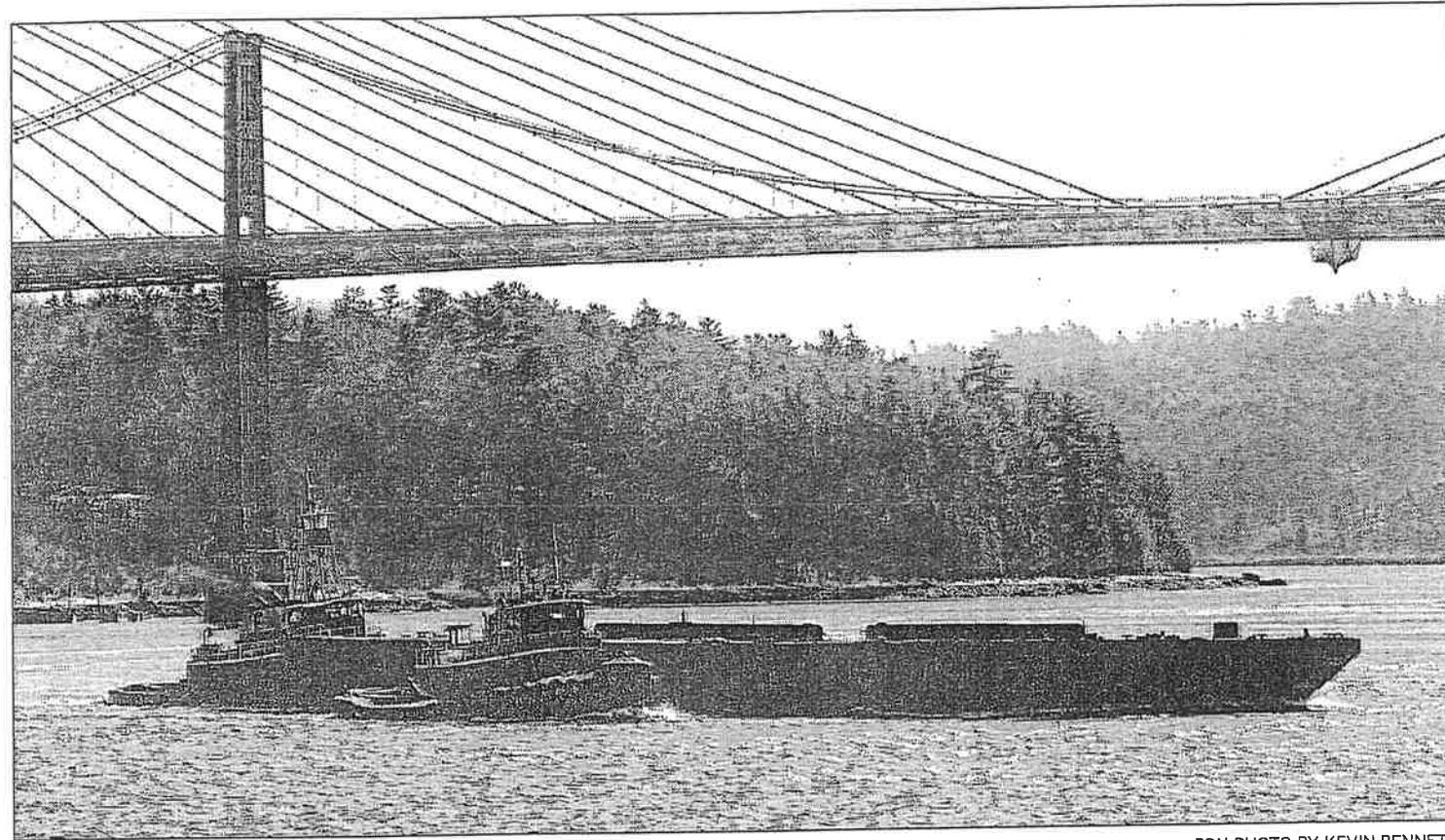
The barge and crane will be positioned underneath the bridge where, in the coming months, pieces of the bridge will be cut away and lowered onto the ship, which will bring them to the Verona Island side of the river, said Philip Roberts, the Maine Department of Transportation's head engineer on the project.

There, another crane will lift the debris off the barge and drop it at a demolition area, where crews will break it into small pieces to be trucked away and recycled or used as construction fill.

The demolition work is going mostly according to schedule, Roberts said. But work day cancellations caused by cold weather have resulted in crews making up time on Saturdays, a scheduling maneuver Roberts anticipates to continue through the winter.

Roberts said the deck's removal will be quite literally a balancing act.

"There's a process to go back



BDN PHOTO BY KEVIN BENNETT

Tugboats guide a barge up the Penobscot River past the Waldo-Hancock Bridge on Saturday. The barge will have a crane loaded on it in Brewer and then return to the bridge to assist in the dismantling of the bridge. The Maine Department of Transportation will remove sections of the bridge over the next seven months.

and forth, from one end to the other, to keep the towers balanced," he said. Demolition crews will take pains to ensure that as sections of the bridge come down, the towers aren't weighed down more on one side than the other, which could test

the strength of the nearly century-old steel.

Once the concrete road surface and steel trusses are gone, workers will begin removing the bridge's main suspension cables and, finally, the two 206-foot towers.

Last week, workers began drilling 16-foot holes into the concrete piers that anchor the towers. Each hole will be fitted with additional supports to ensure the towers withstand the demolition process.

Roberts said the process of re-

inforcing the towers is a slow one: "Some of the concrete is rotten and some is good, so getting the core out has been rough," he said.

Crews could not begin to take down the bridge until late fall, when endangered peregrine falcons and osprey, which nest on

and near the bridge, left for warmer skies.

In recent years, two osprey nests have been relocated from the bridge to nest stands on either side of the Penobscot River. A third nest will be relocated to the Verona Island side by the time the birds return in the spring. A falcon nest on the west tower will be relocated to a cliff face in Prospect.

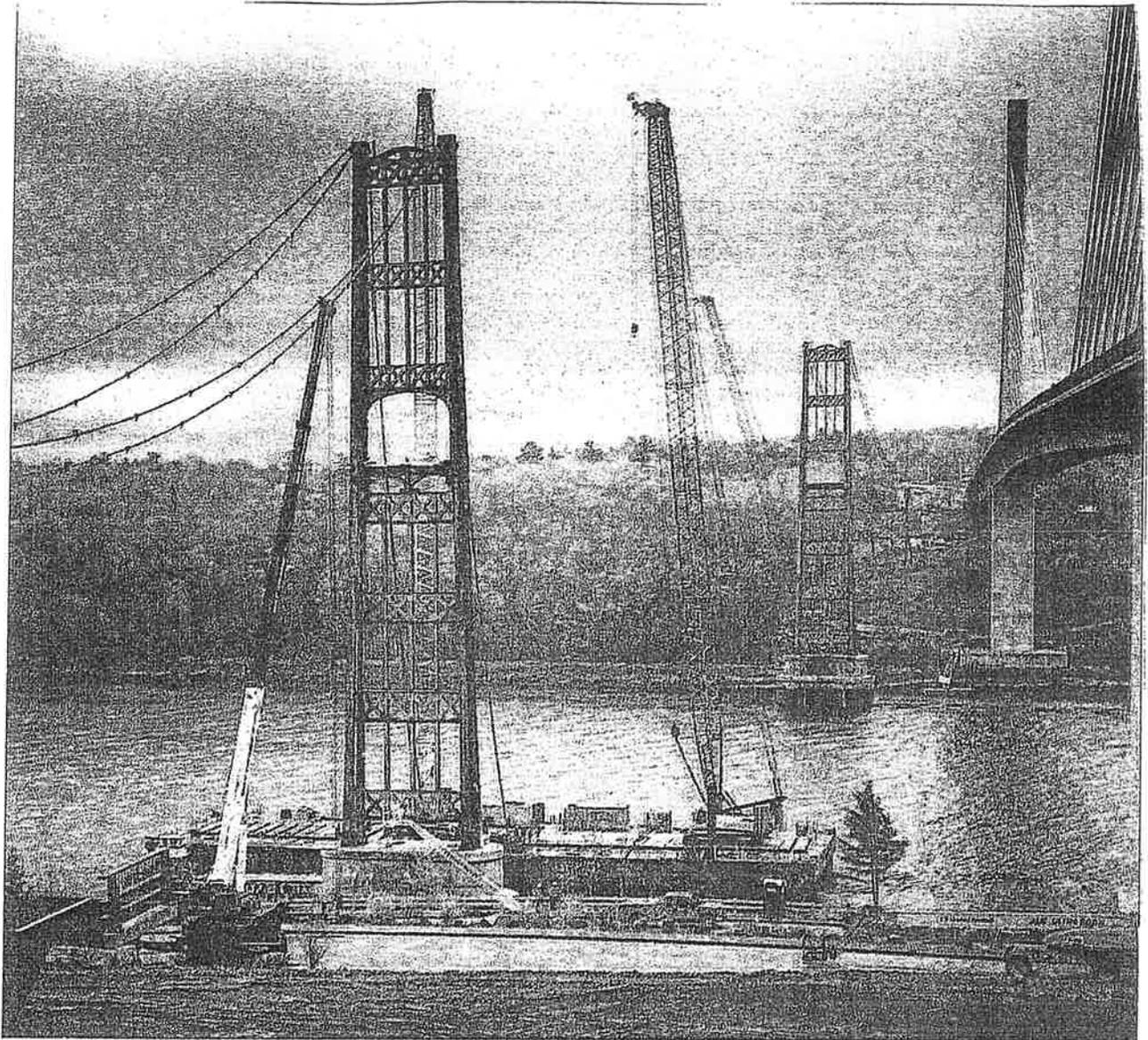
Roberts said all the demolition work will take place off-road, meaning the removal of the old bridge shouldn't impact traffic on the busy U.S. Route 1.

Sentimentalists and historians also will be happy that the 81-year-old bridge won't disappear completely: The concrete piers that support the bridge's towers will remain in the water and the construction of a "historic viewing area" on the Prospect side, complete with information about both bridges, mean the bridge won't be easily forgotten.

Plus, Verona Island and Prospect will each get a piece of the bridge to call their own: Both will receive one of the flagpoles that topped the bridge. The poles were removed in December and are being sandblasted and readied for display.

Roberts also said curious onlookers will have plenty of time to check out the bridge's demolition, from viewing areas at the banks of either county.

"It'll be a very slow process, so if anyone's interested in seeing it, we welcome them to come down," he said.



BDN PHOTO BY MARIO MORETTO

Workers from S&R Corp., of Lowell, Mass., cut support cables from the two enormous towers of the Waldo-Hancock Bridge on Thursday.

5/17/13 BDN

# Cutting the last cables

## Lines drop from old Waldo-Hancock Bridge

BY MARIO MORETTO  
BDN STAFF

Watch the video  
[www.bangordailynews.com](http://www.bangordailynews.com)

VERONA ISLAND — With a thunderous, steely groan and an enormous splash, the two towers of the defunct Waldo-Hancock Bridge were separated for the first time since 1931 after demolition workers dropped the suspension cables into the Penobscot River.

From a platform held aloft by a crane more than ten stories above the river, workers for S&R Corporation of Lowell, Mass., used a torch to split the last remaining cables between the

two towers. The cables were originally added to the bridge in 2000, to supplement the original steel cords that held up the deck below.

As the 900-foot-long supplemental cables — each made of 2-inch galvanized steel, bundled into packs of four — were split, they fell into the river below. The clang of steel hitting steel as the cables struck the towers, and the giant splash as they plunged into the river, could be heard from

nearby Fort Knox.

"I'm sure that after the first one dropped, any fish in the area were gone for quite awhile," said Phil Roberts, Maine Department of Transportation's resident engineer for the bridge demolition.

After the cables fell, a barge moved to each tower to remove the portion of the cables that was underwater. The remaining length will be removed before the final phase of deconstruction — the two 206-foot towers — begins next week.

As the crew worked Thursday, idle  
*See Bridge, Page B6*

# Bridge

*Continued from Page B1*

workers and nearby residents gathered on the Penobscot Narrows Bridge, which replaced the Waldo-Hancock in 2006, to watch the cables fall.

"It's the end of a great bridge," said Larry Wahl, owner of Wahl's Dairy Port in Bucksport, who brought a camera to document the demolition. "It's a shame to see it go, but it had to be done."

Wahl said he has been coming to the new bridge from time to time to watch each step of the old one's demise. Earlier this spring, the last piece of the Waldo-Hancock's 2,000-foot deck was removed, leaving the bridge a ghostly skeleton of its former self.

Wahl said that aside from feeling sentimental about the bridge slowly withering away, he also came Thursday to admire the men working in the crane platform high above the water.

"I used to be in construction, and I'm fascinated by crane work," he said. "I'm like a little

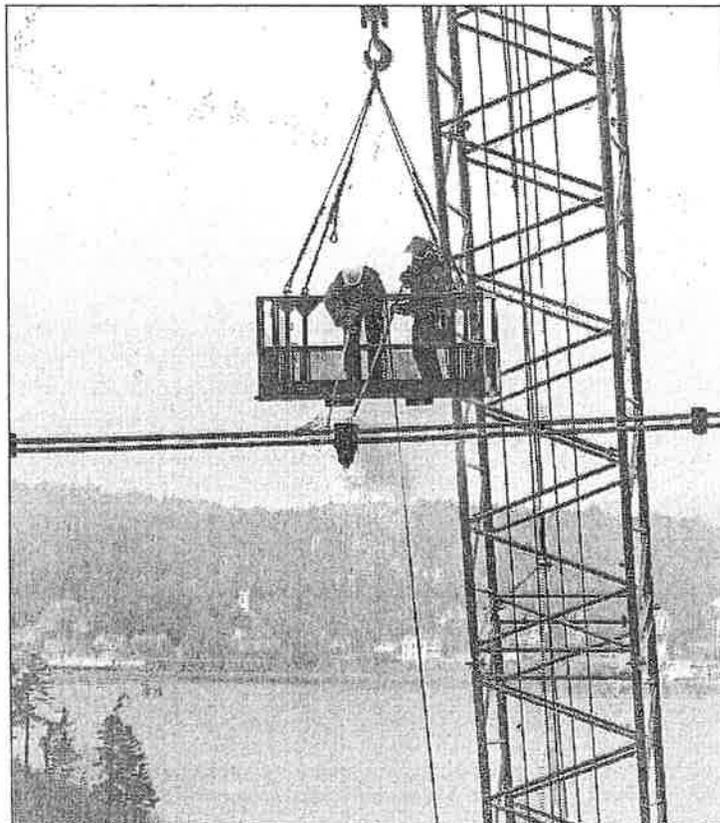
kid with an erector set."

Work to disassemble the 82-year-old bridge began in November. The towers will come down last. Each will be cut into sections, starting from the top, which will be lowered by crane, and then cut into sections, starting from the top, which will be lowered by crane until all that's left are the concrete piers in the river.

In the early 20th century, the Waldo-Hancock Bridge cost \$846,000 to build. That's \$12.77 million in today's dollars. The Penobscot Narrows Bridge — which was speedily built while the state implemented stop-gap measures to temporarily strengthen the older, failing bridge — cost \$85 million to build and is 2,120 feet long. Its towers are 440 feet high.

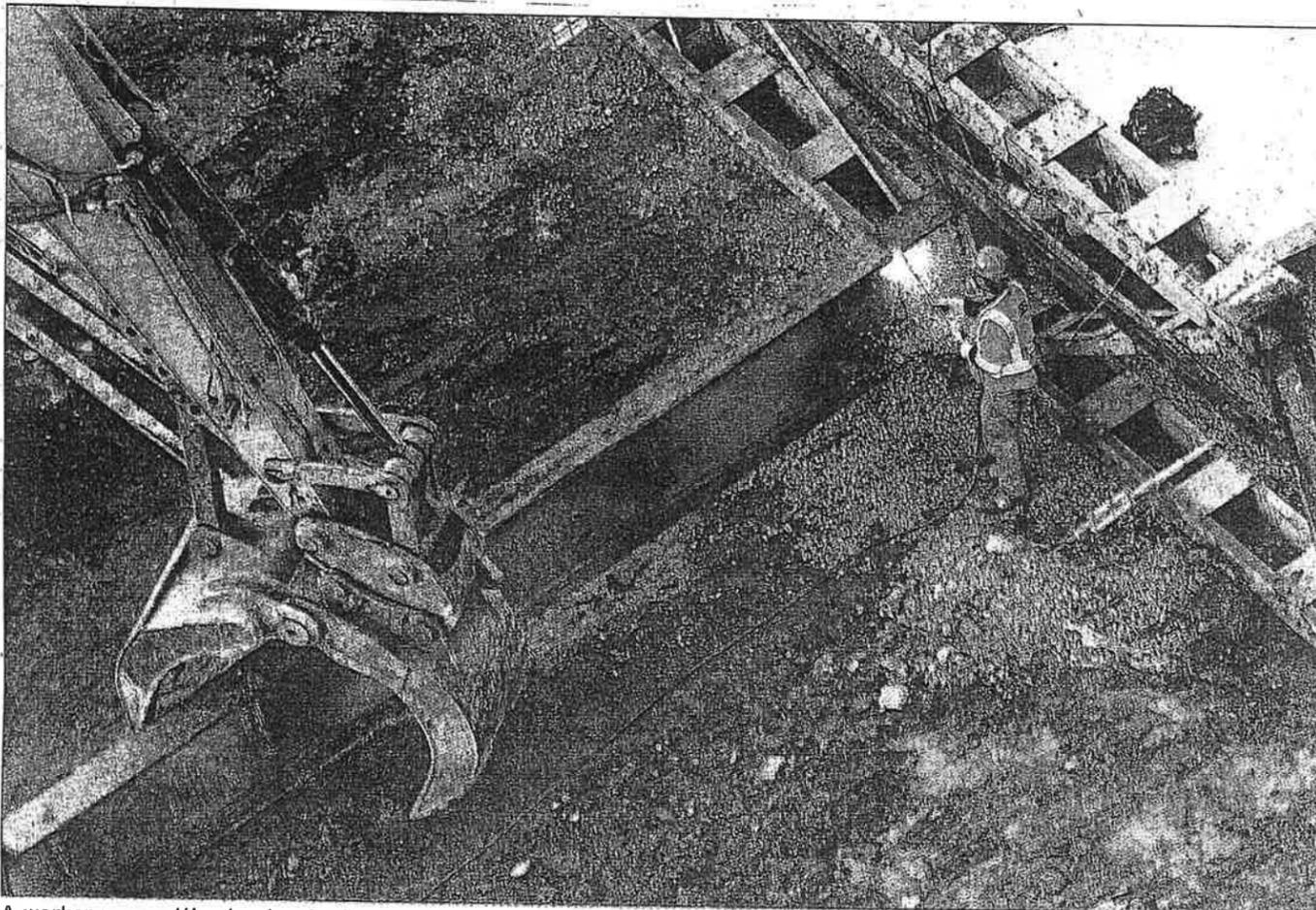
Demolition work was contracted to S&R for \$5.35 million. Work also is underway on a historical outlook on the Prospect side of the bridge, where visitors will have scenic views of the Penobscot River, downtown Bucksport and the newer bridge. Eventually, placards will be placed detailing the history of both bridges.

Follow Mario Moretto on Twitter at @riocarmine.



BDN PHOTO BY MARIO MORETTO

Workers for S&R Corp., of Lowell, Mass., use a torch to split a supplemental cable bundle on the Waldo-Hancock Bridge on Thursday. The cables crashed into the river below, from which they were retrieved hours later.



A worker uses cutting torches to dismantle a section of the Waldo-Hancock Bridge recently. Small sections are being cut out and lowered by a crane. Crews alternate sides to try to keep the weight balanced on the two towers as the bridge is dismantled.

# Remembering Waldo-Hancock Bridge

Text and photos by  
Kevin Bennett

BDN  
4/17/13

The first time I can recall crossing the Waldo-Hancock Bridge between Verona Island and Prospect as a child was on a trip to visit my grandparents in New Haven, Conn. The crossing was both amazing and terrifying for a young boy who had never seen or crossed such a span.

I wondered each time we made the trip if the bridge was safe. It seemed so narrow and, in my young mind, it seemed to sway a bit as my dad piloted the family van carrying five of us over it.

I came to recognize the bridge as a marking point on our trips, a gateway to a new adventure. And seeing the green metal structure as it loomed on the horizon coming up Route 1, returning to Mount Desert Island, it was a "welcome home" sign.

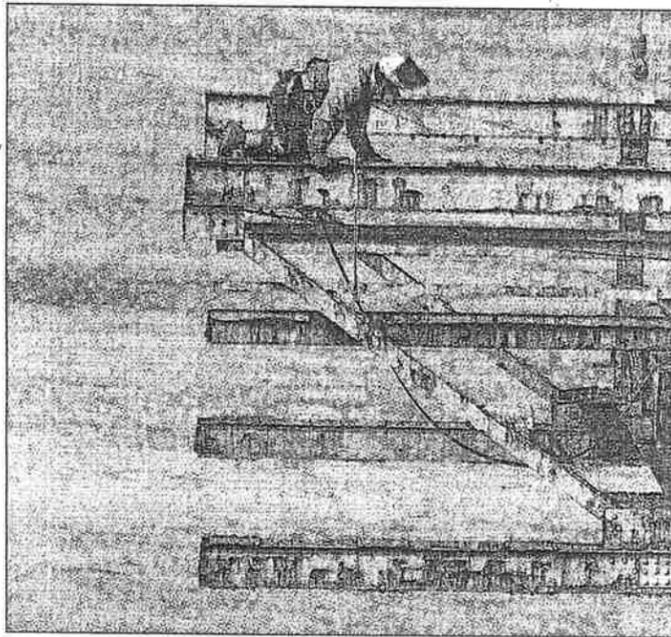
Alan S. Grover of Monroe also remembers the span as a highlight of his trips to Corea with family. He remembers the toll booth at the bridge, where 35 cents bought you passage across the 2,040-foot span of steel and concrete. Standing on the Prospect side talking with MDOT engineer Phil Roberts, Grover said he couldn't see the need for a new bridge, thinking the old bridge was just fine.

Engineers in 2003 discovered corrosion in the cables hidden by the sheaths covering them. The damage had been done, the corrosion had reached a point of no return and a new bridge was needed.

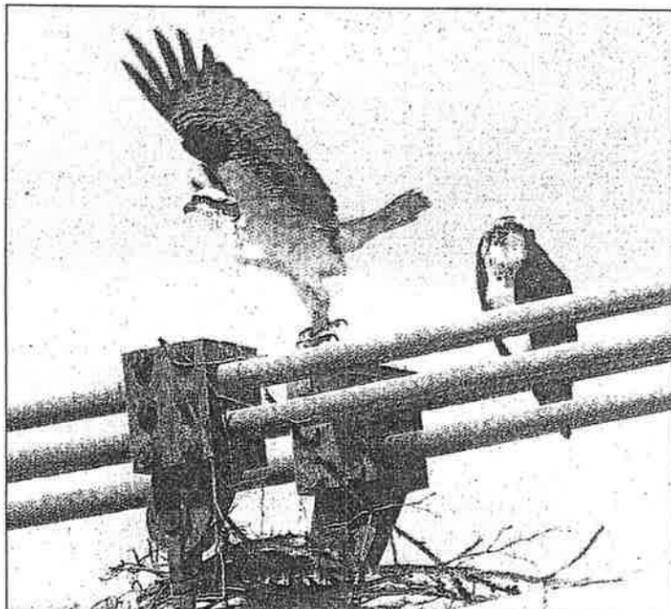
Dec. 30, 2006, marked the opening of the new Penobscot Narrows Bridge, which led to the dismantling of the old Waldo-Hancock Bridge.

Delayed by a lack of funding, the demolition is now taking place. Workers are methodically removing sections and allowing the bridge to balance itself as it slowly is reduced to a pile of scrap metal and ground concrete.

When the dust has settled, the Waldo-Hancock Bridge will be just a memory. Young travelers mostly likely will marvel at the new Penobscot Narrows Bridge as they travel across. It will become the marking point for the start of many an adventure and the welcome sign to many returning home.



A worker crawls back to his crew after watching a steel section of the Waldo-Hancock Bridge be lowered by a crane to the deck of a waiting barge as it floats on the Penobscot River.



An osprey takes flight after dropping sticks on a nest being built on the Waldo-Hancock Bridge between Verona Island and Prospect on Monday. The pair of birds seem oblivious to the work crew using cutting torches to dismantle the other side of the bridge.

MAINE  
FRAME